

Fig. 2a
(Prior Art)

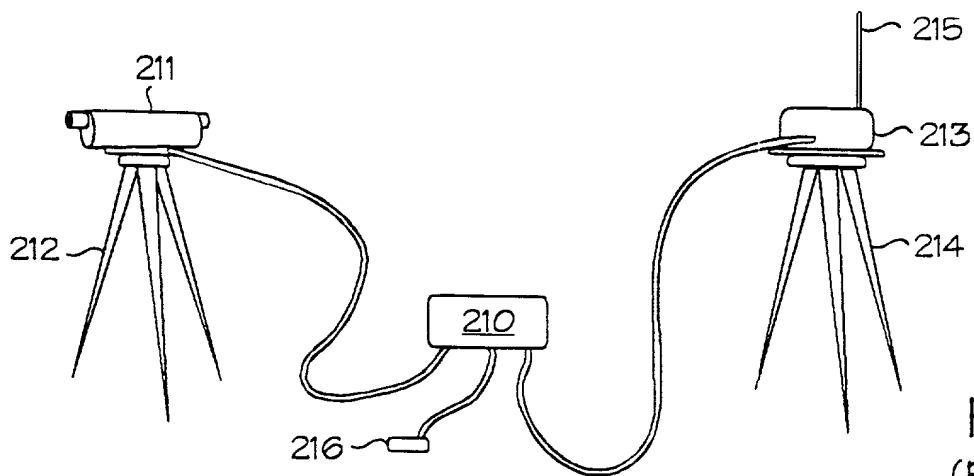


Fig. 2b
(Prior Art)

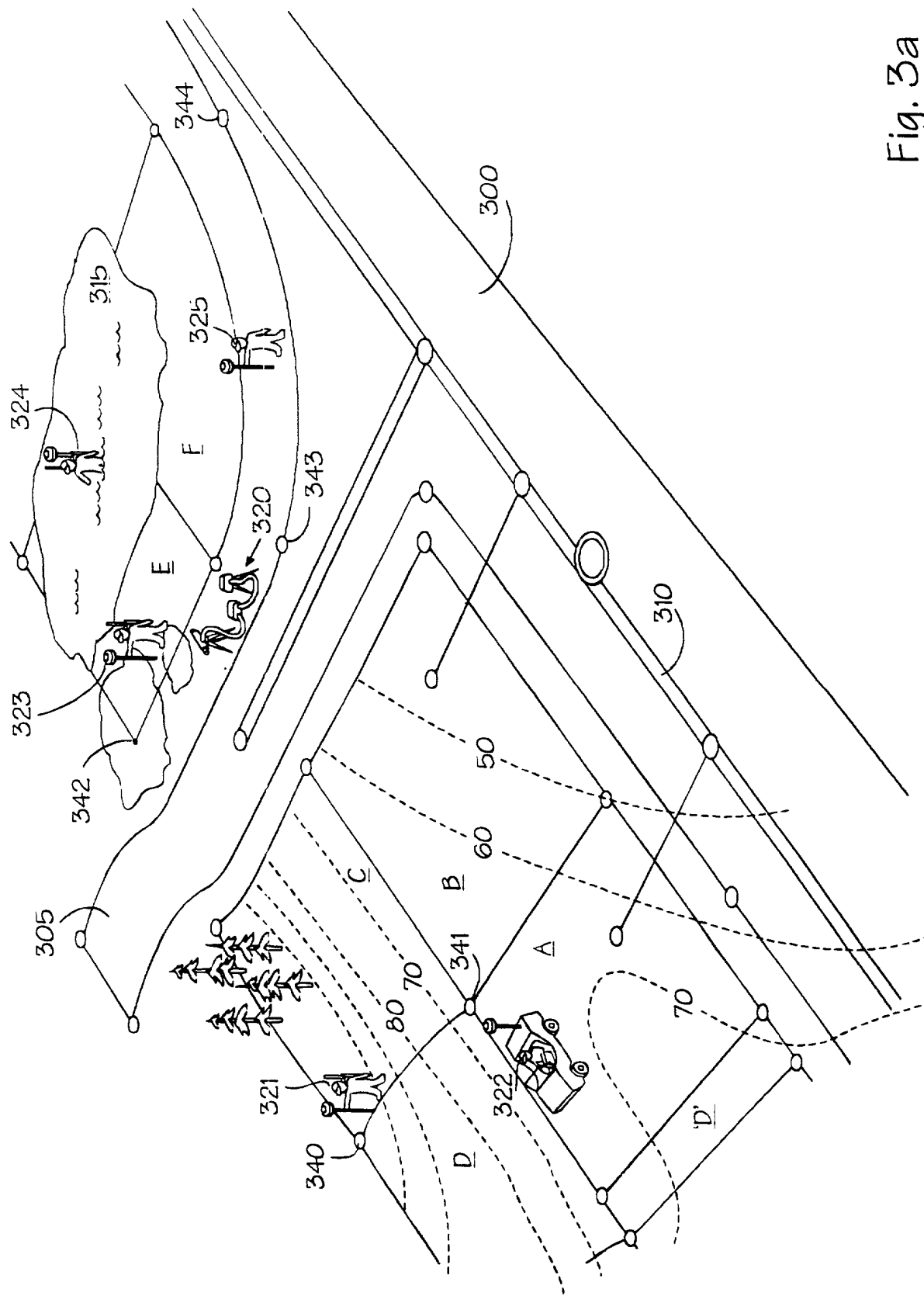


Fig. 3a

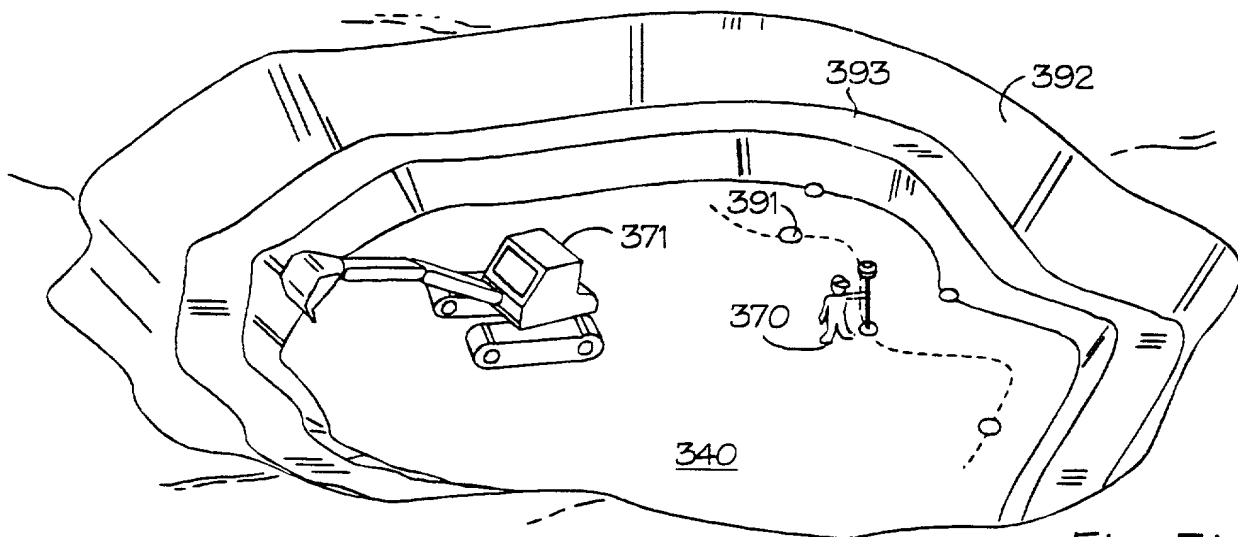
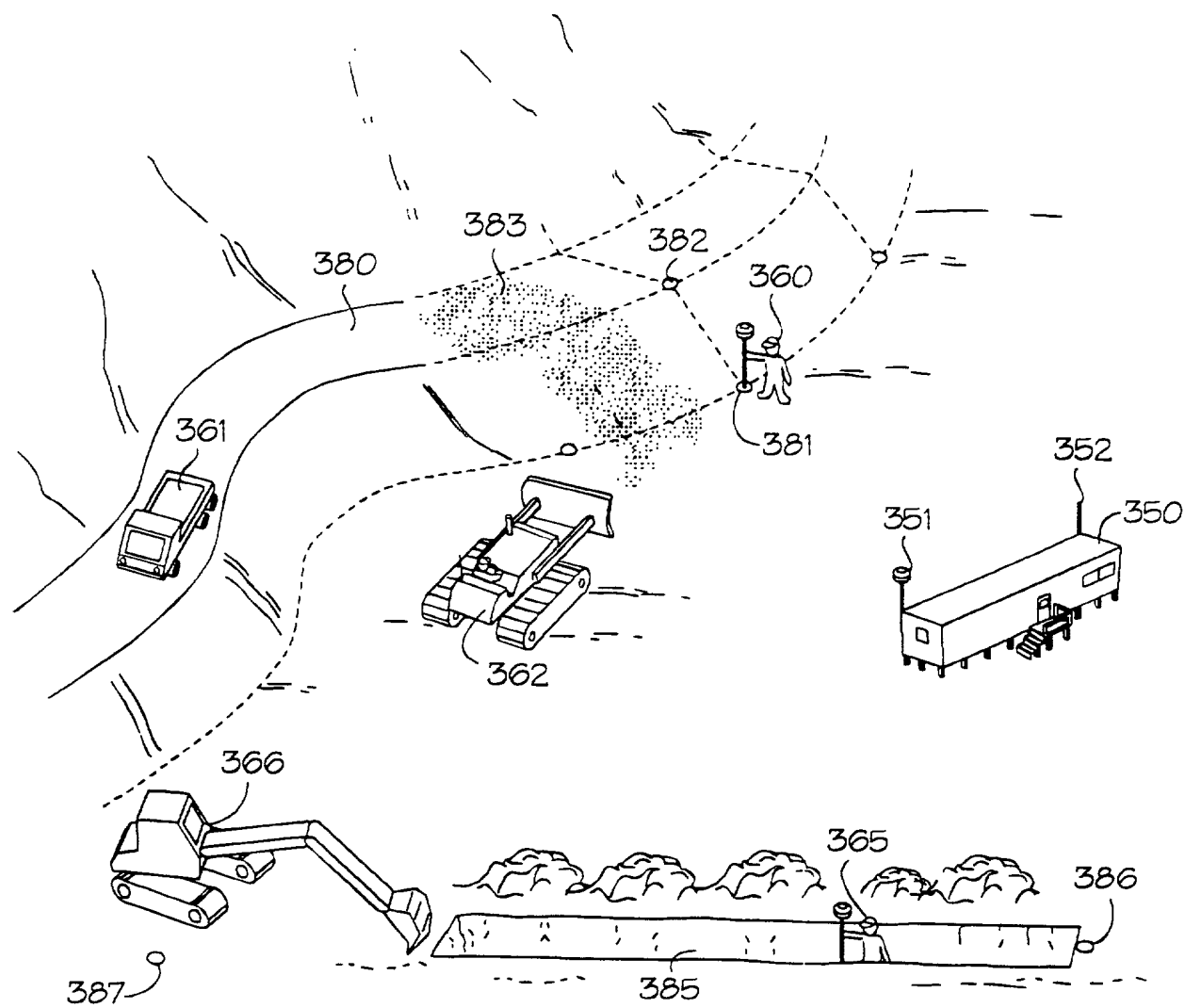


Fig. 3b

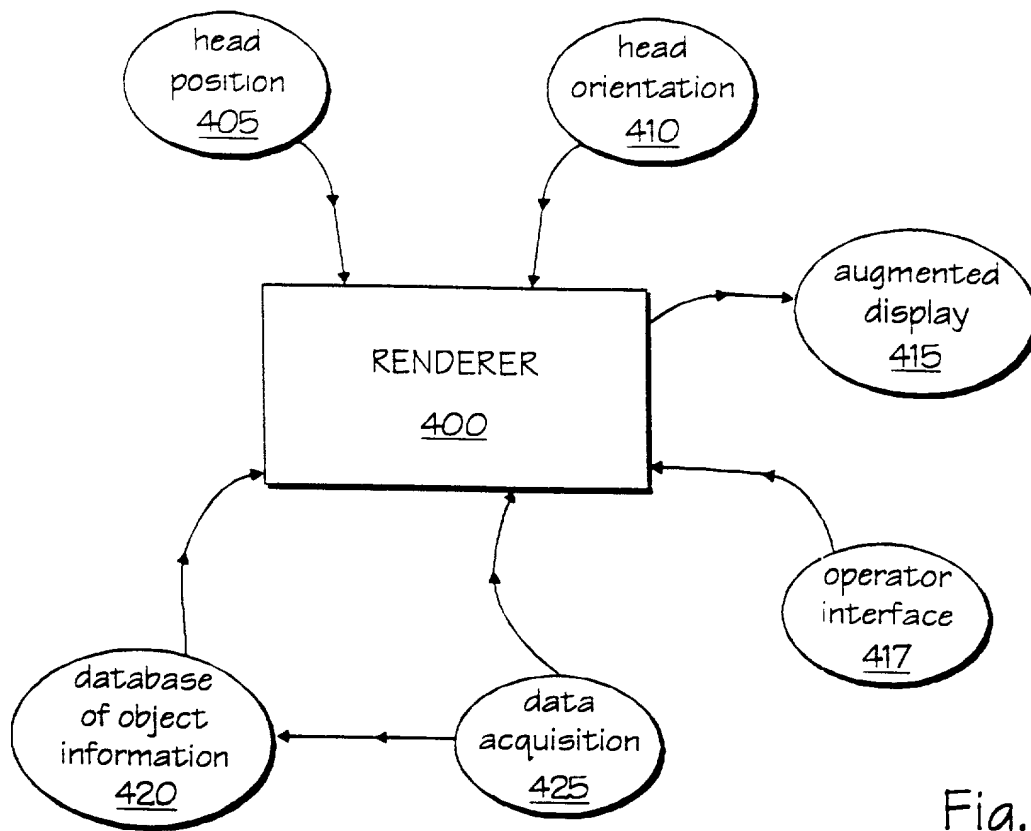


Fig. 4a

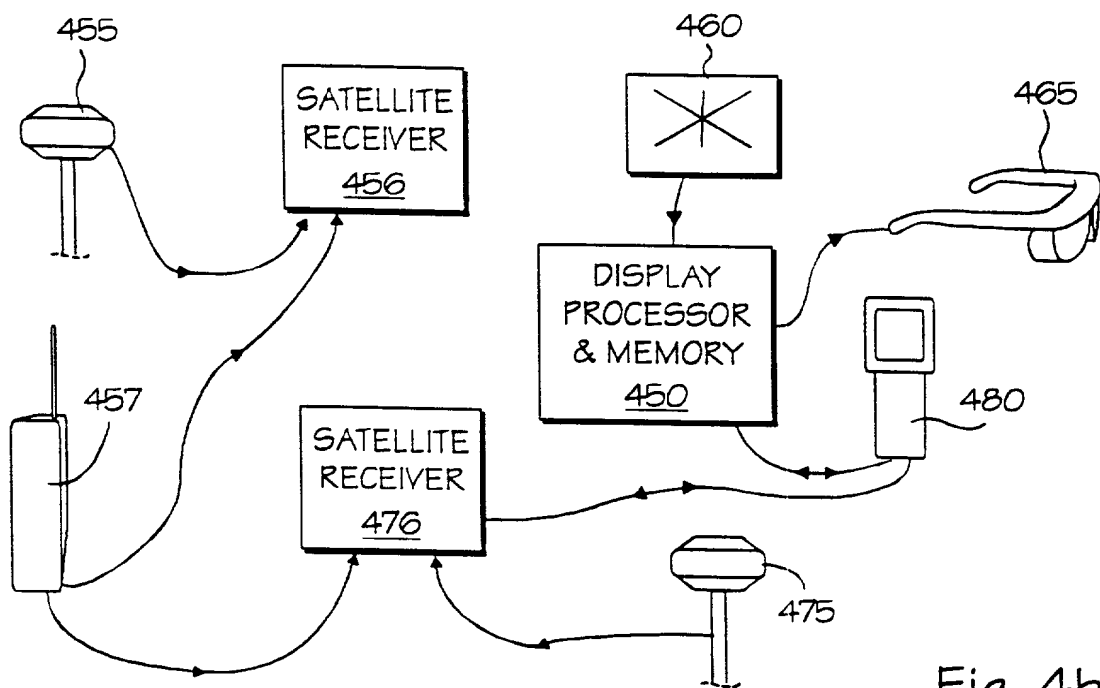


Fig. 4b

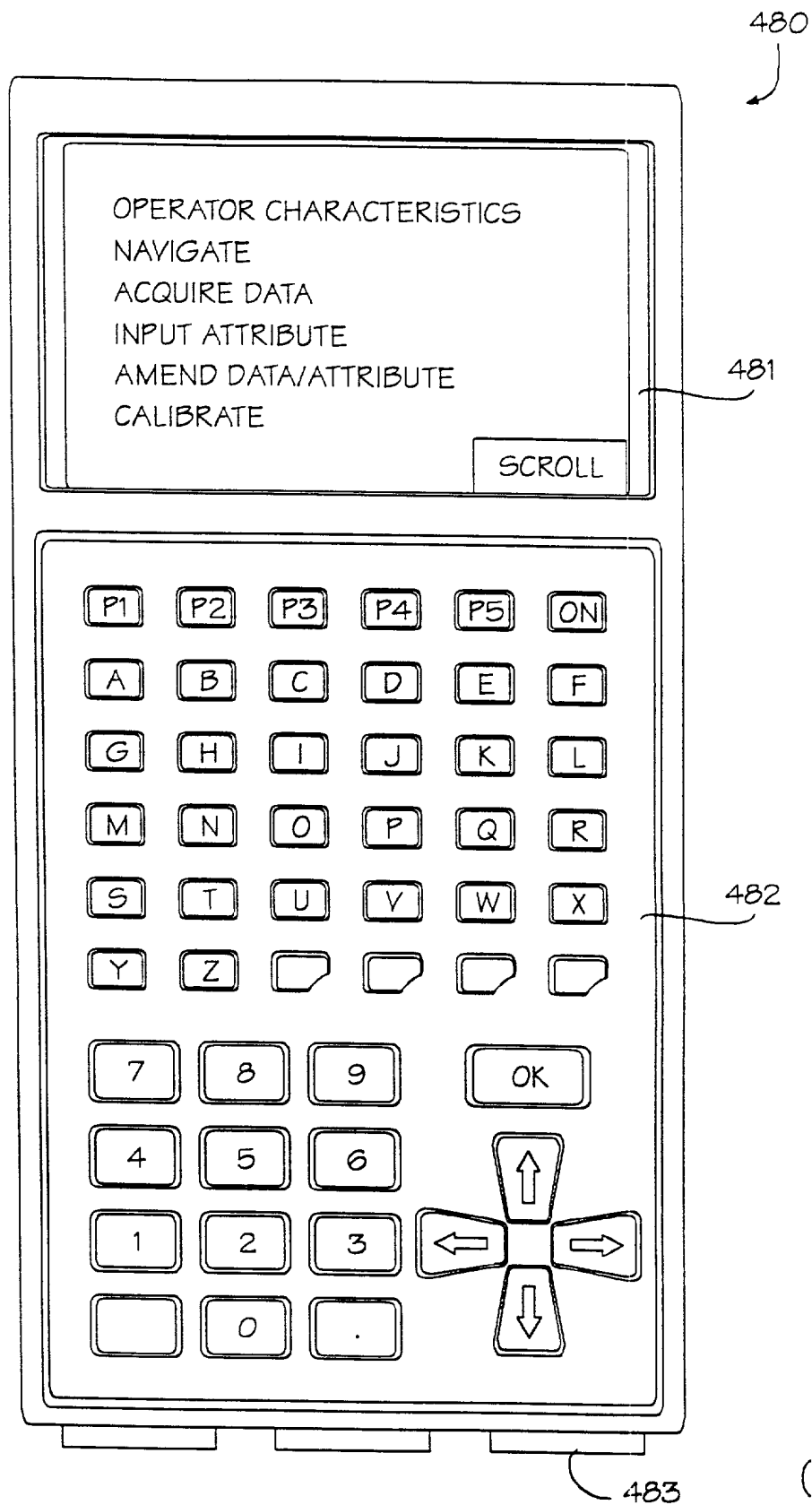


Fig. 4c
(Prior Art)

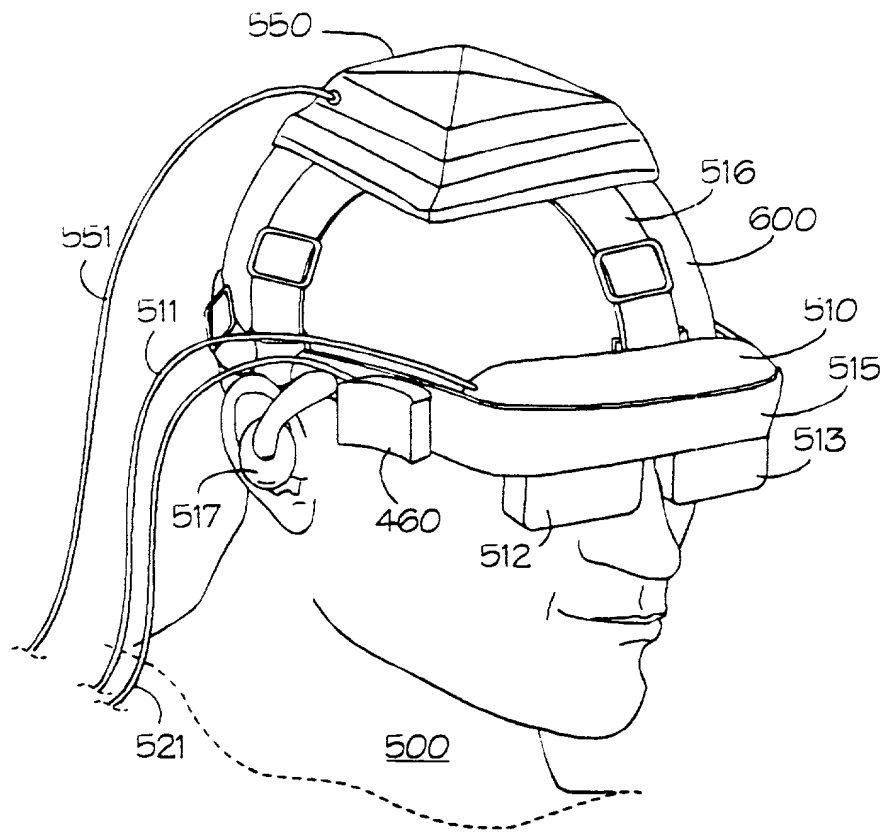


Fig. 5a

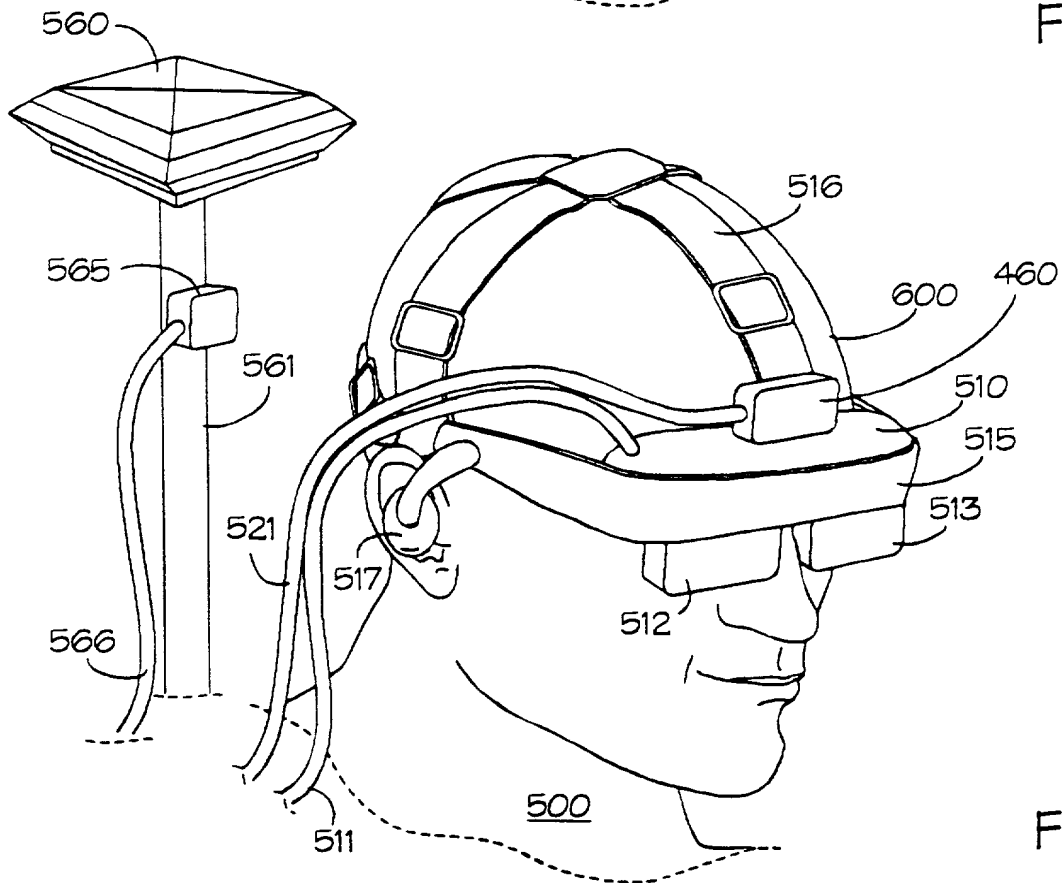


Fig. 5b

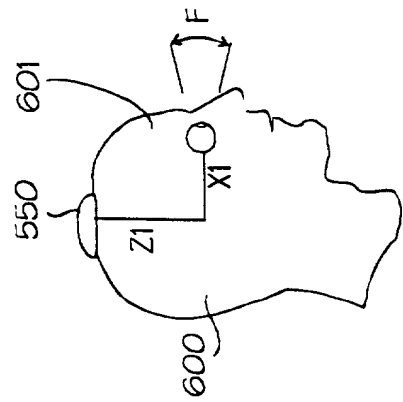


Fig. 6a

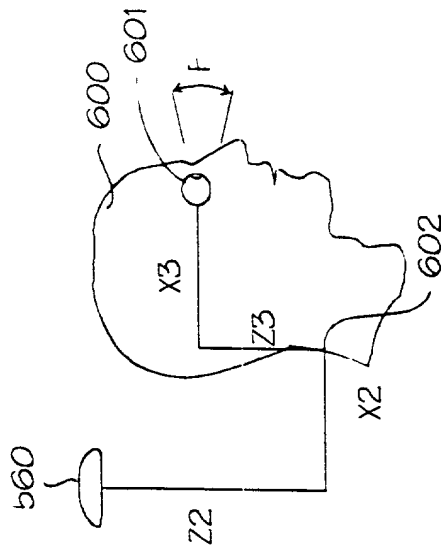
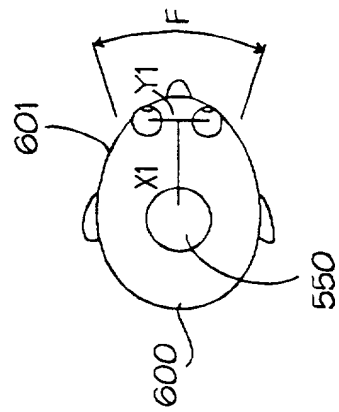
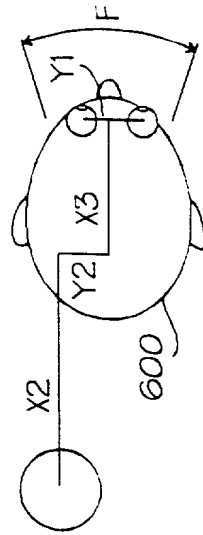
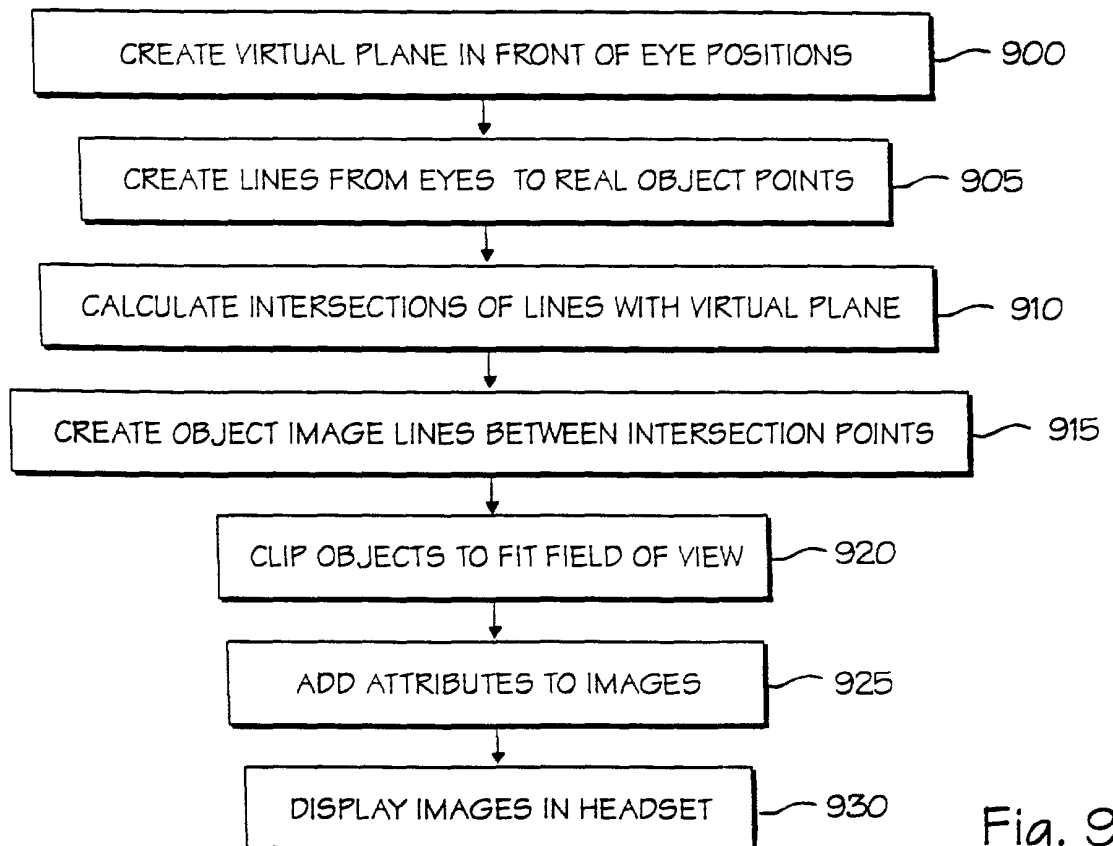
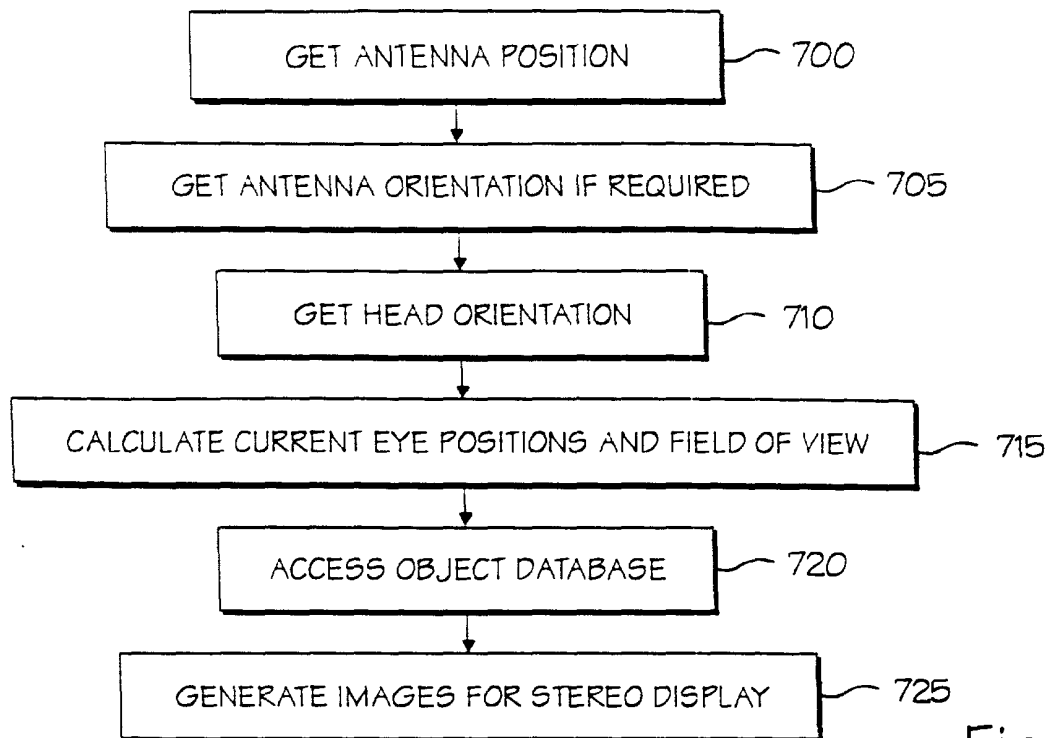


Fig. 6b





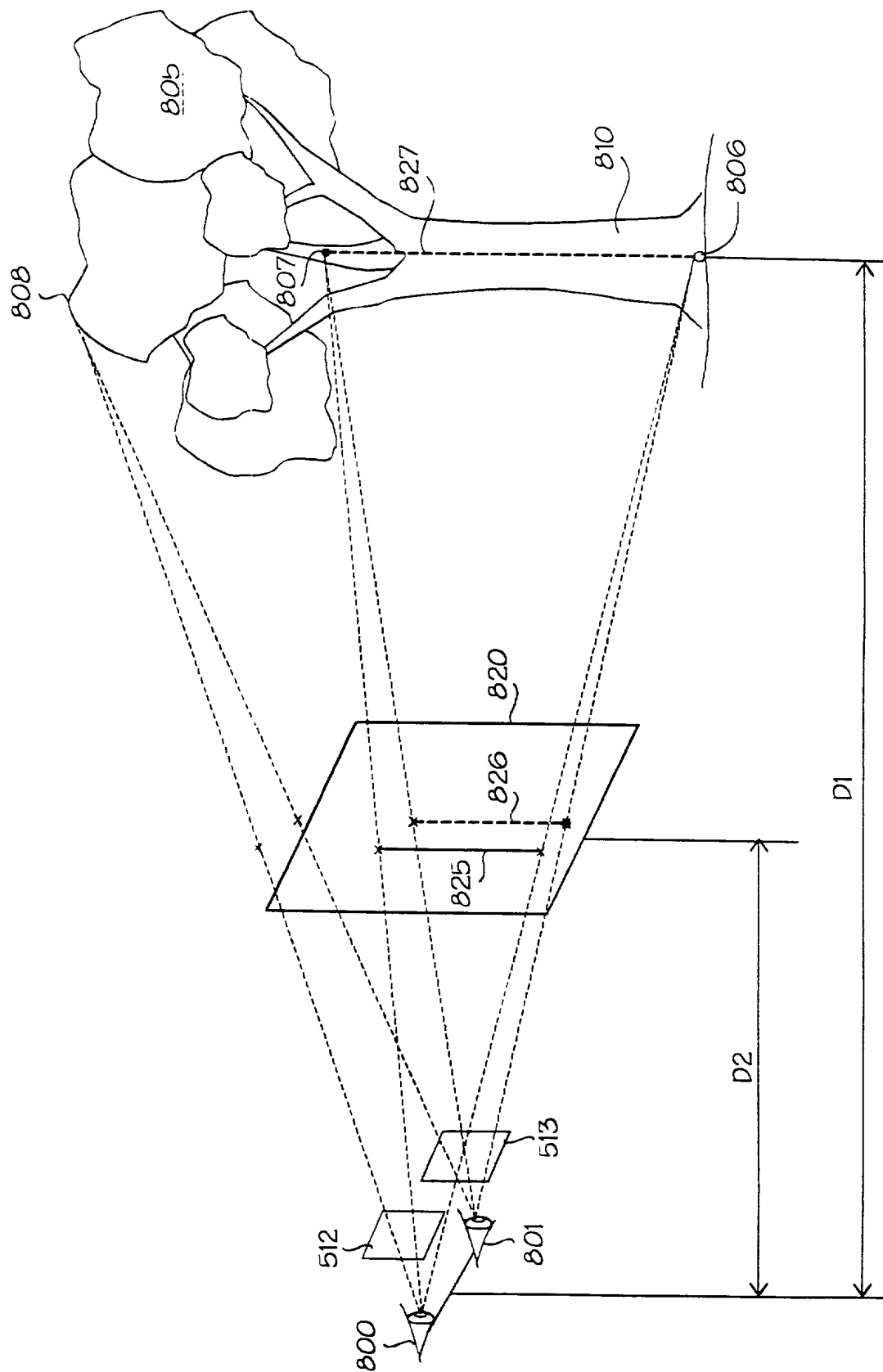


Fig. 8

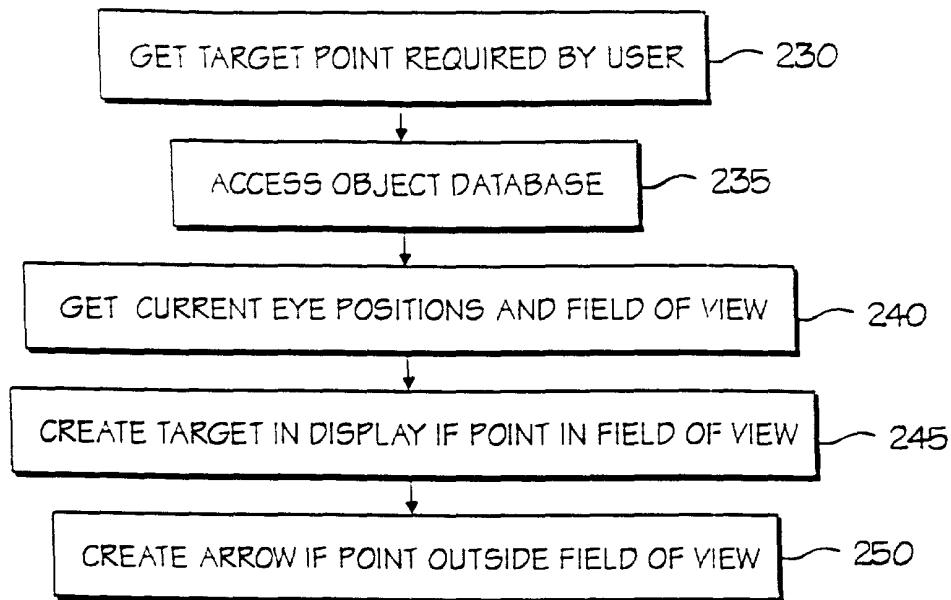


Fig. 12

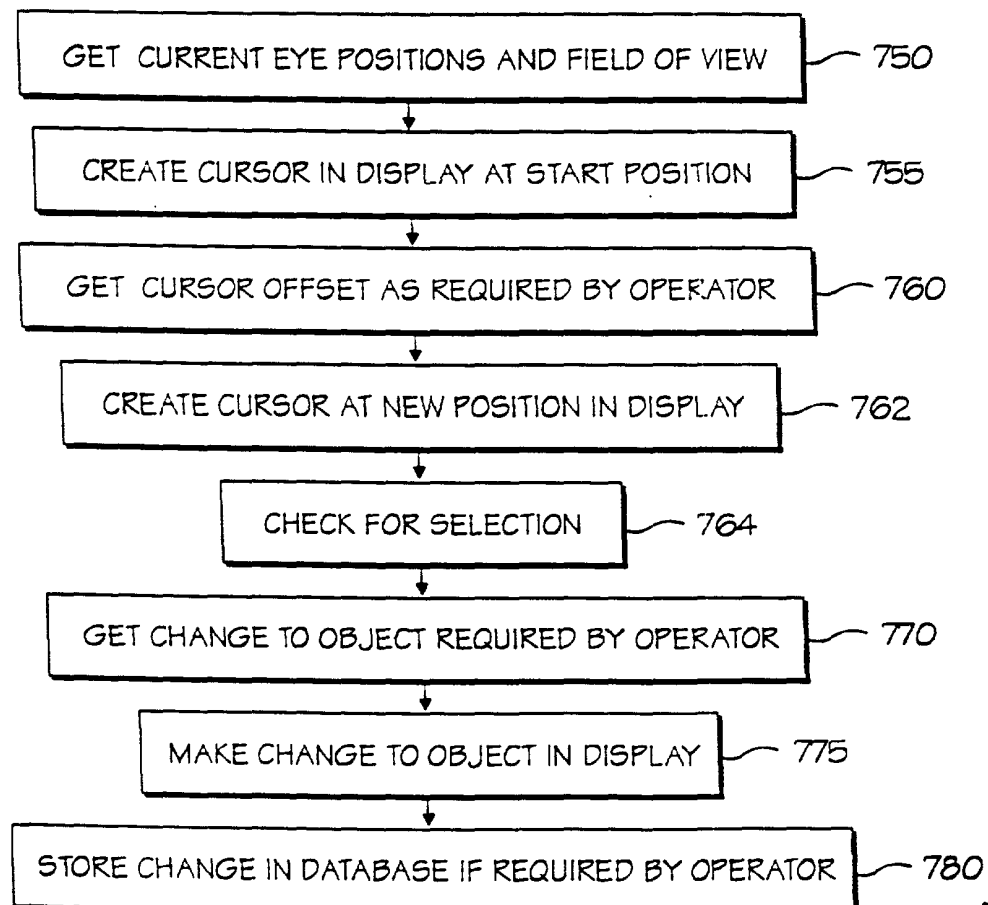


Fig. 14

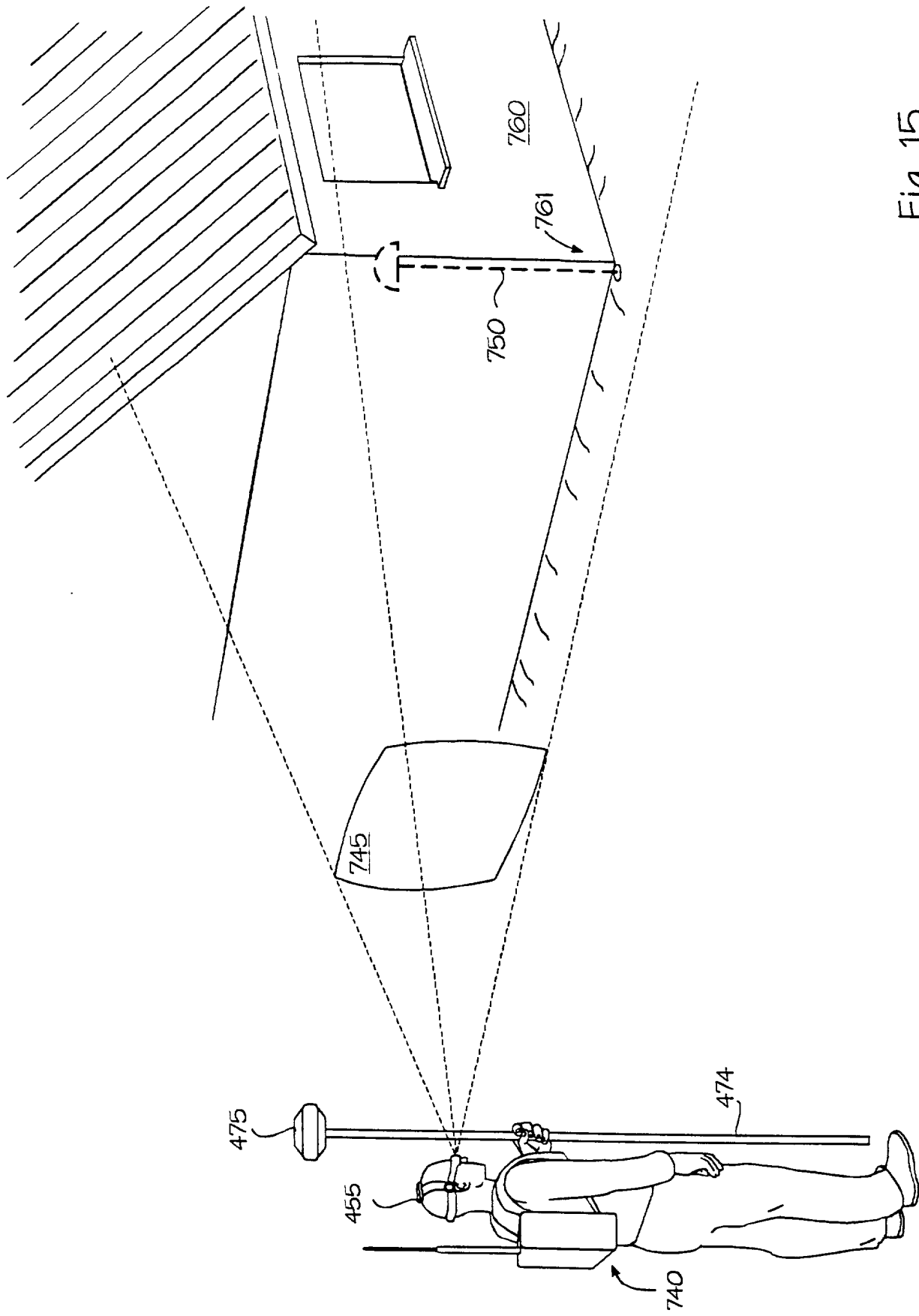


Fig. 15

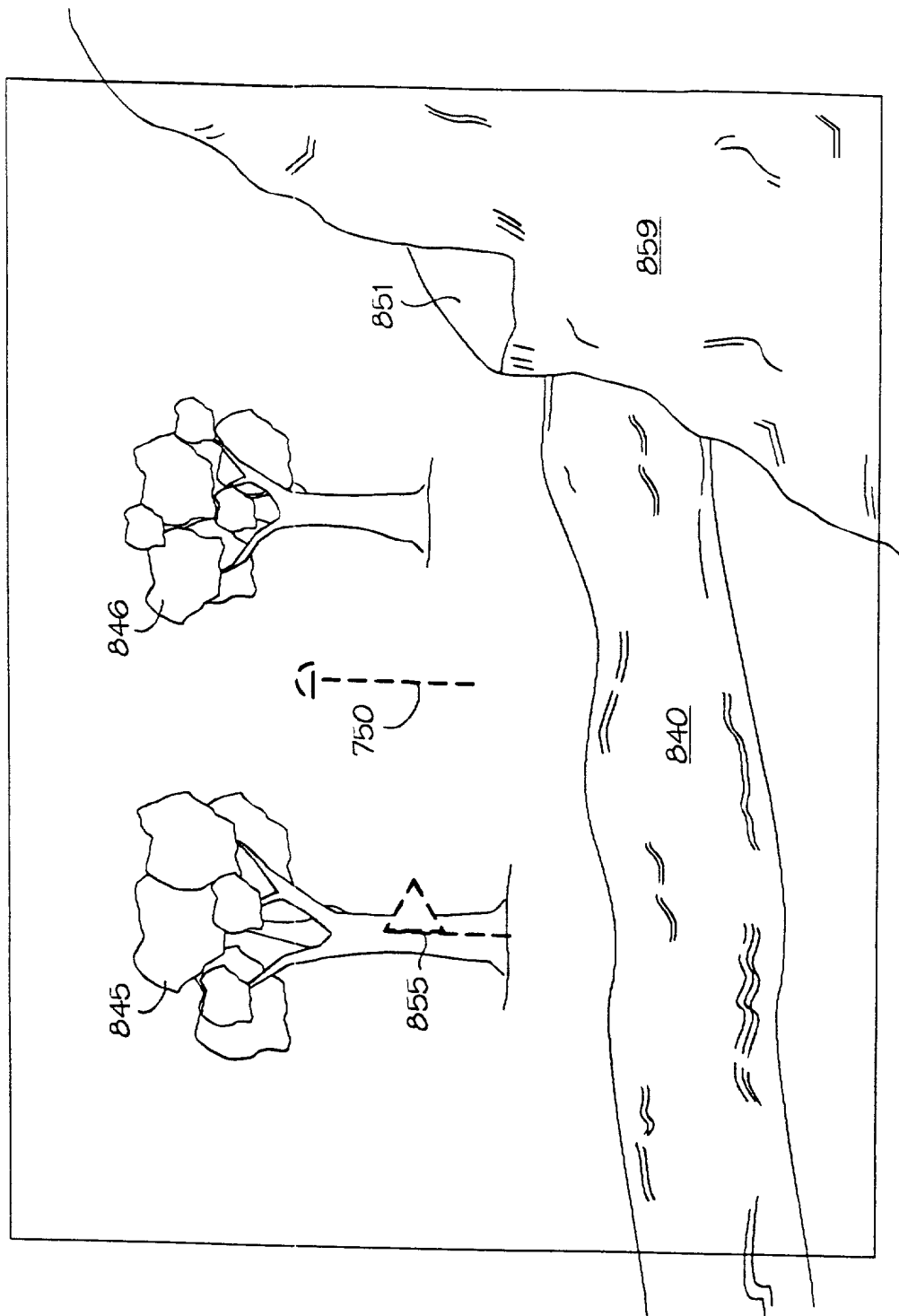


Fig. 16

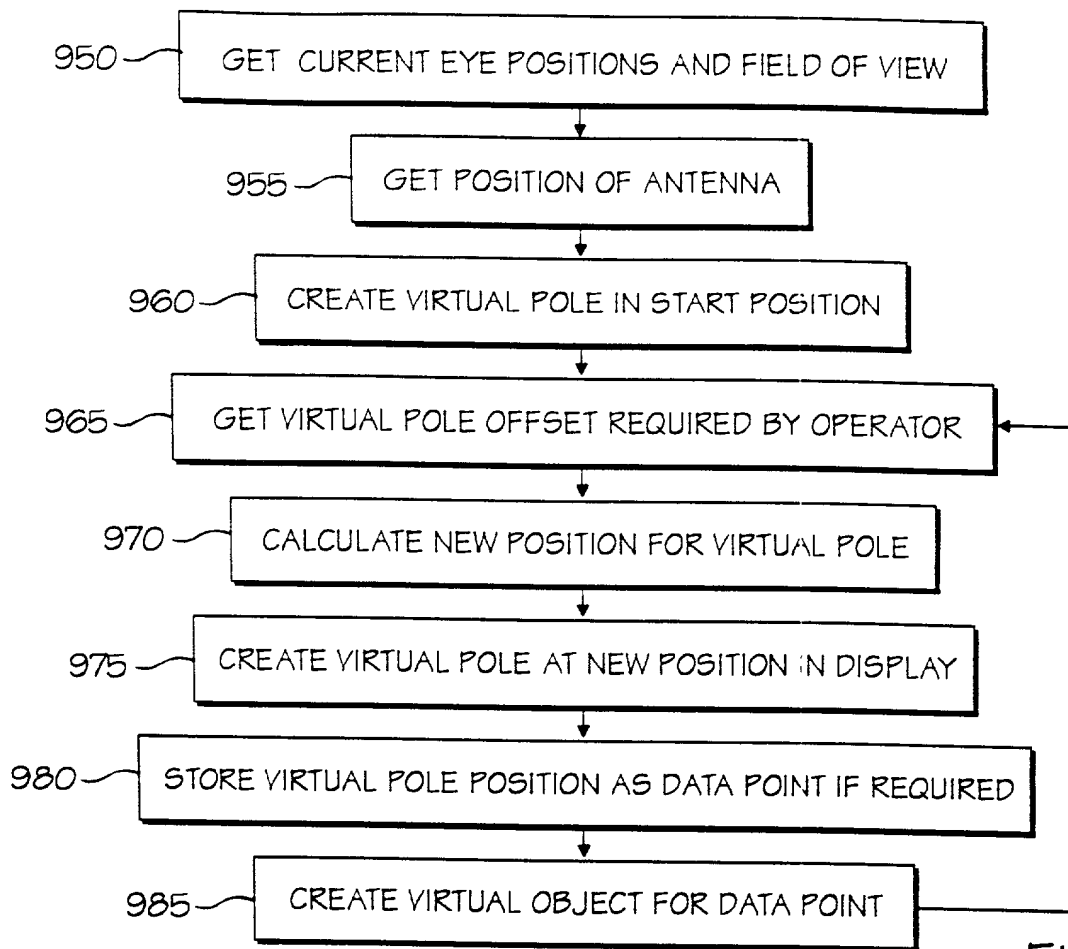


Fig. 17

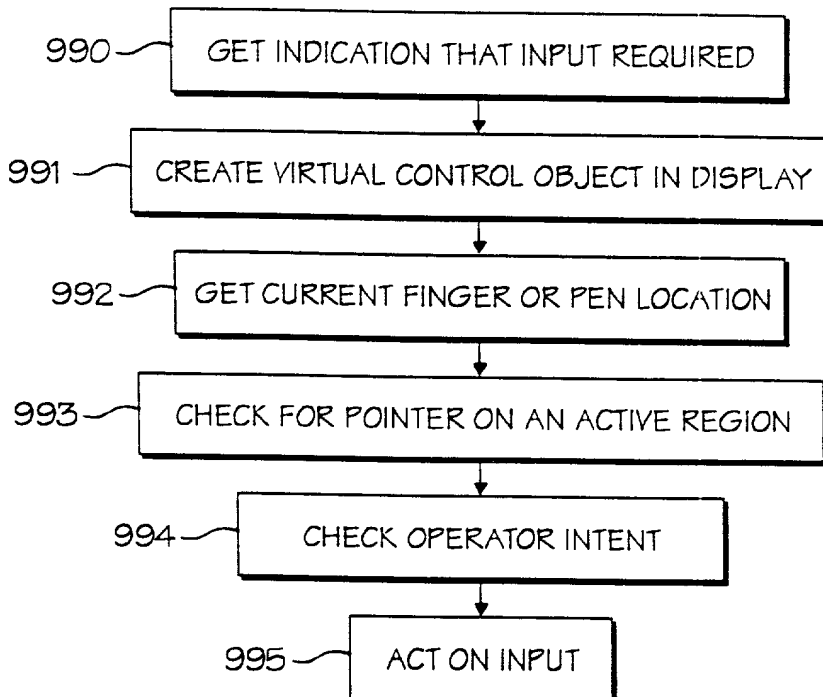


Fig. 20

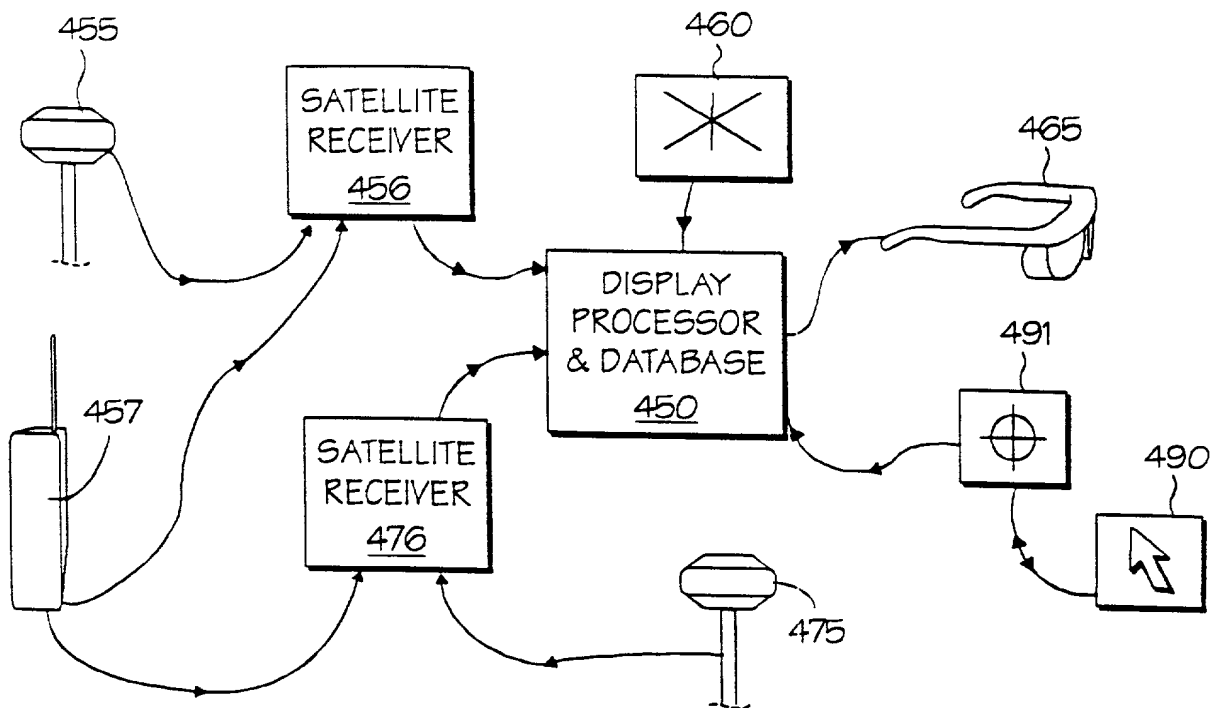


Fig. 18

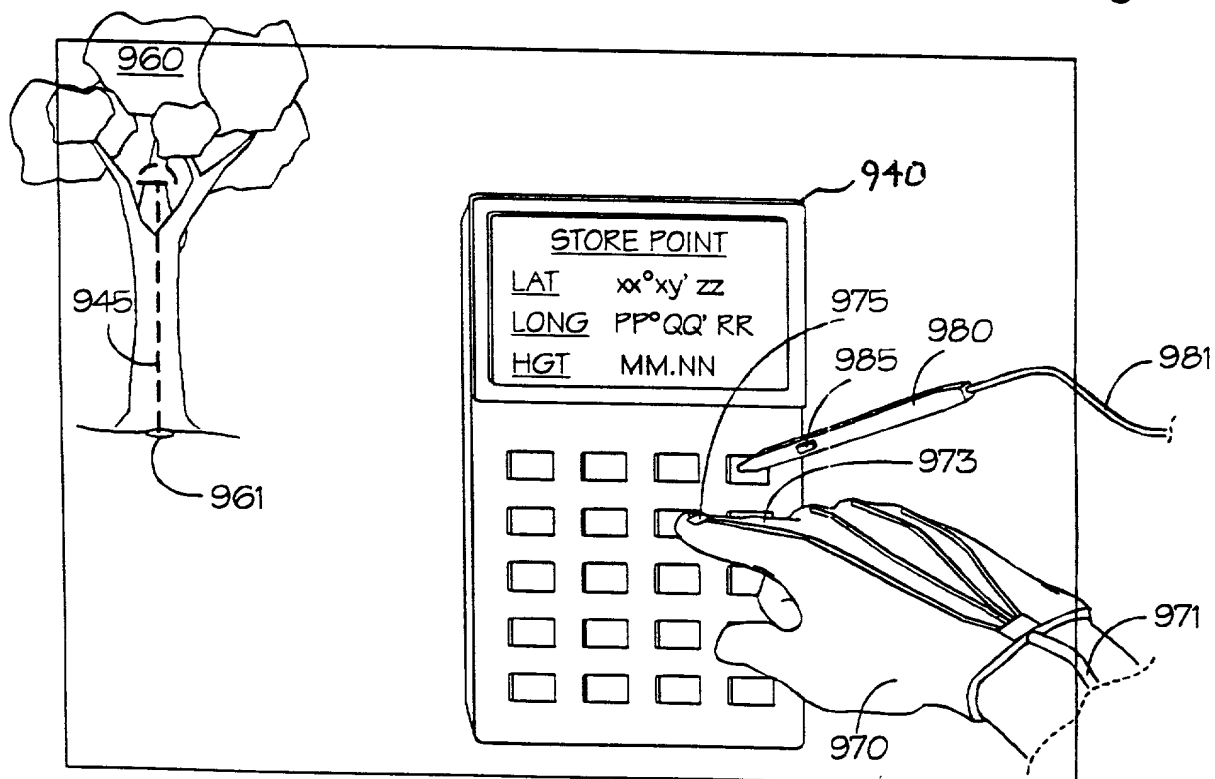


Fig. 19

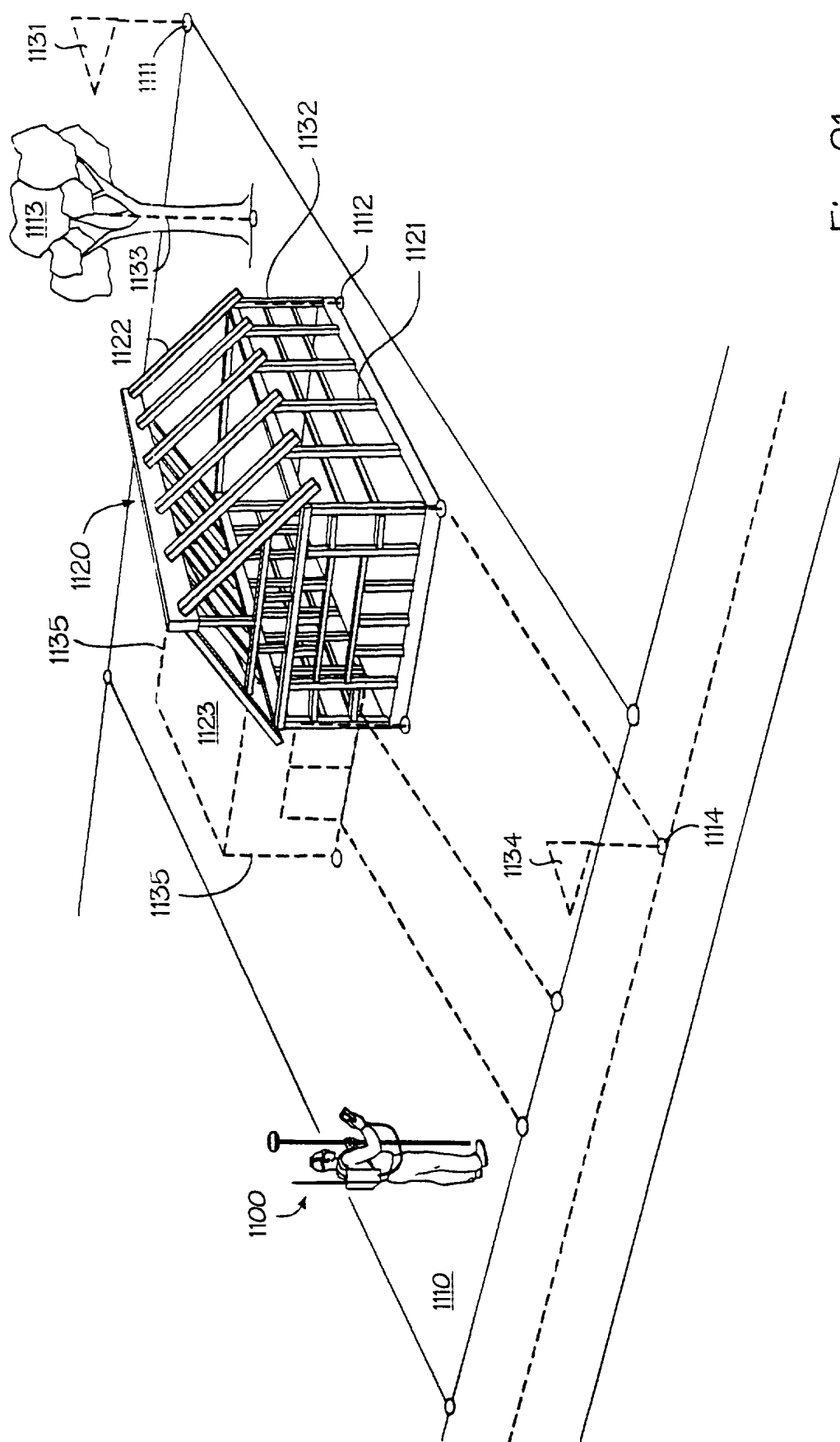


Fig. 21

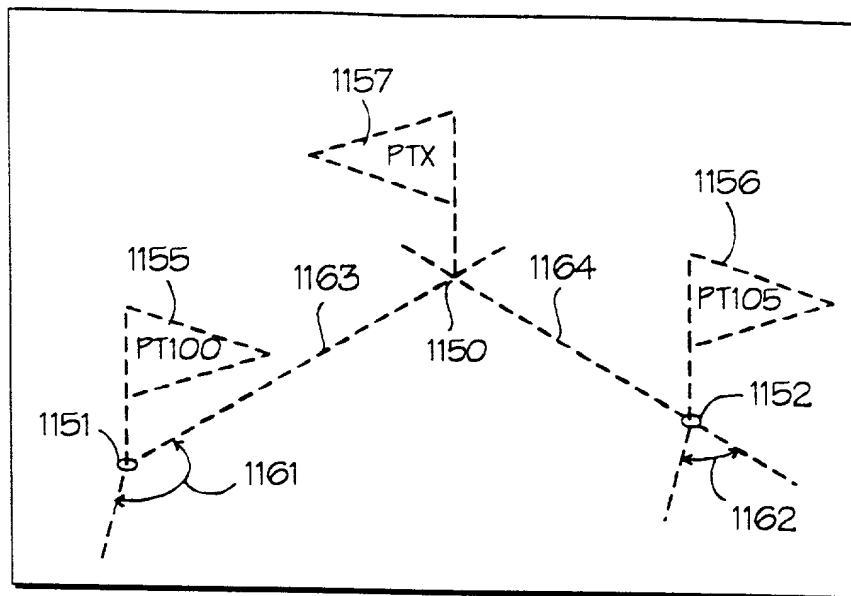


Fig. 22

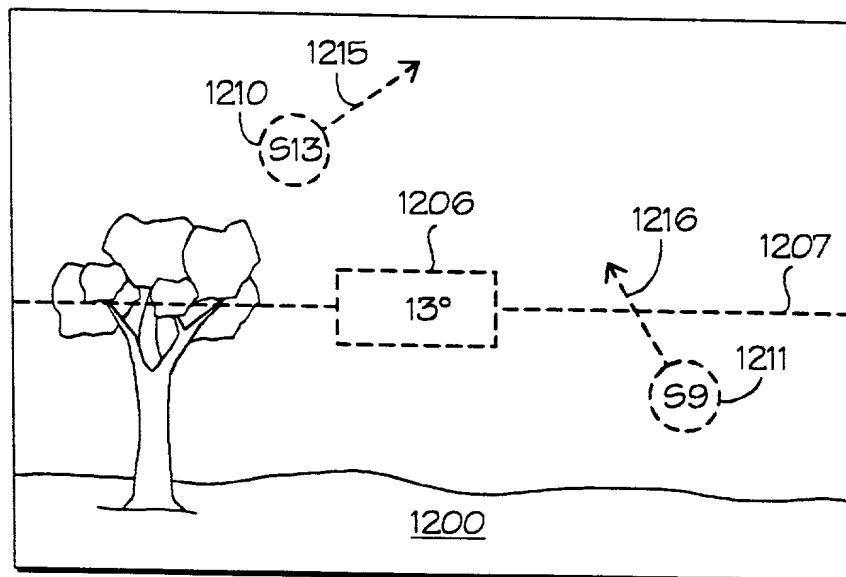


Fig. 25

INTERSECTION OF BEARINGS

| | |
|---------|-------|
| POINT 1 | PT100 |
| AZIMUTH | 170° |
| POINT 2 | PT105 |
| AZIMUTH | 80° |

CALC

Fig. 23a

INTERSECTION RESULT

| | |
|-----------|--------|
| RESULT | PTX |
| NORTH | 7.75 m |
| EAST | 8.45 m |
| ELEVATION | 1.5 m |

STORE DISPLAY

Fig. 23b

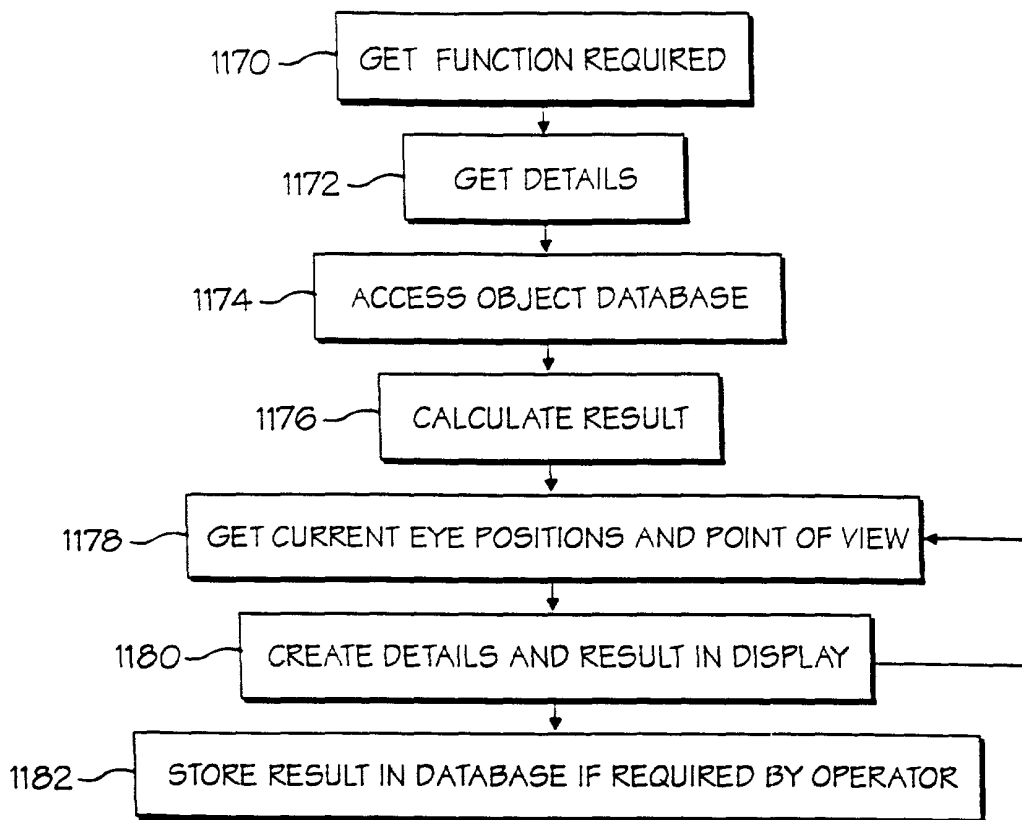


Fig. 24

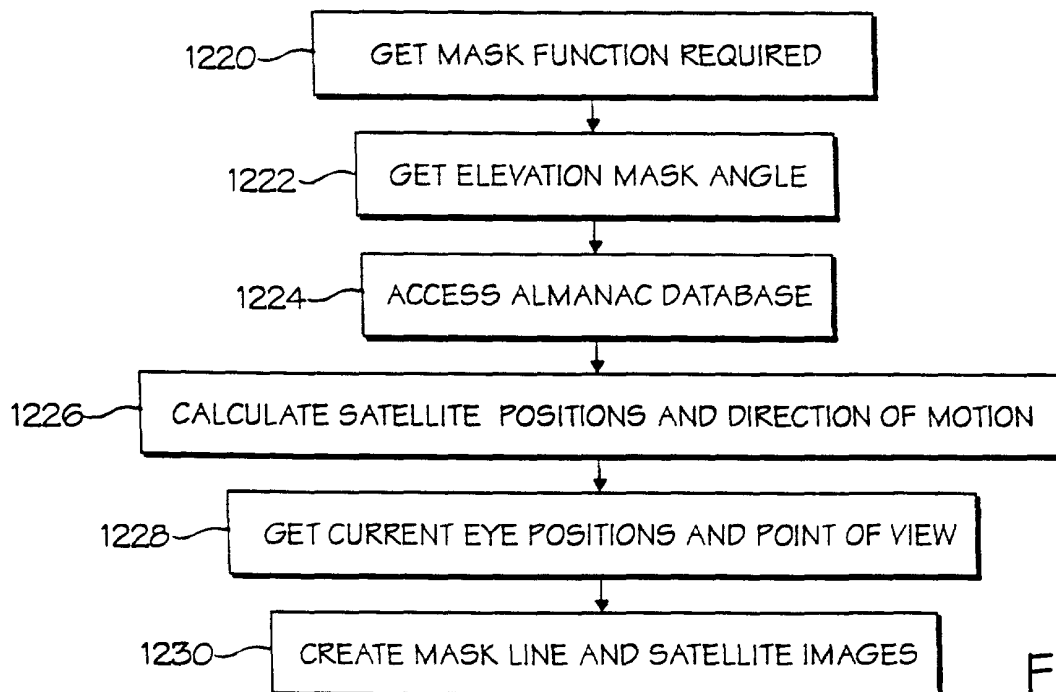


Fig. 26

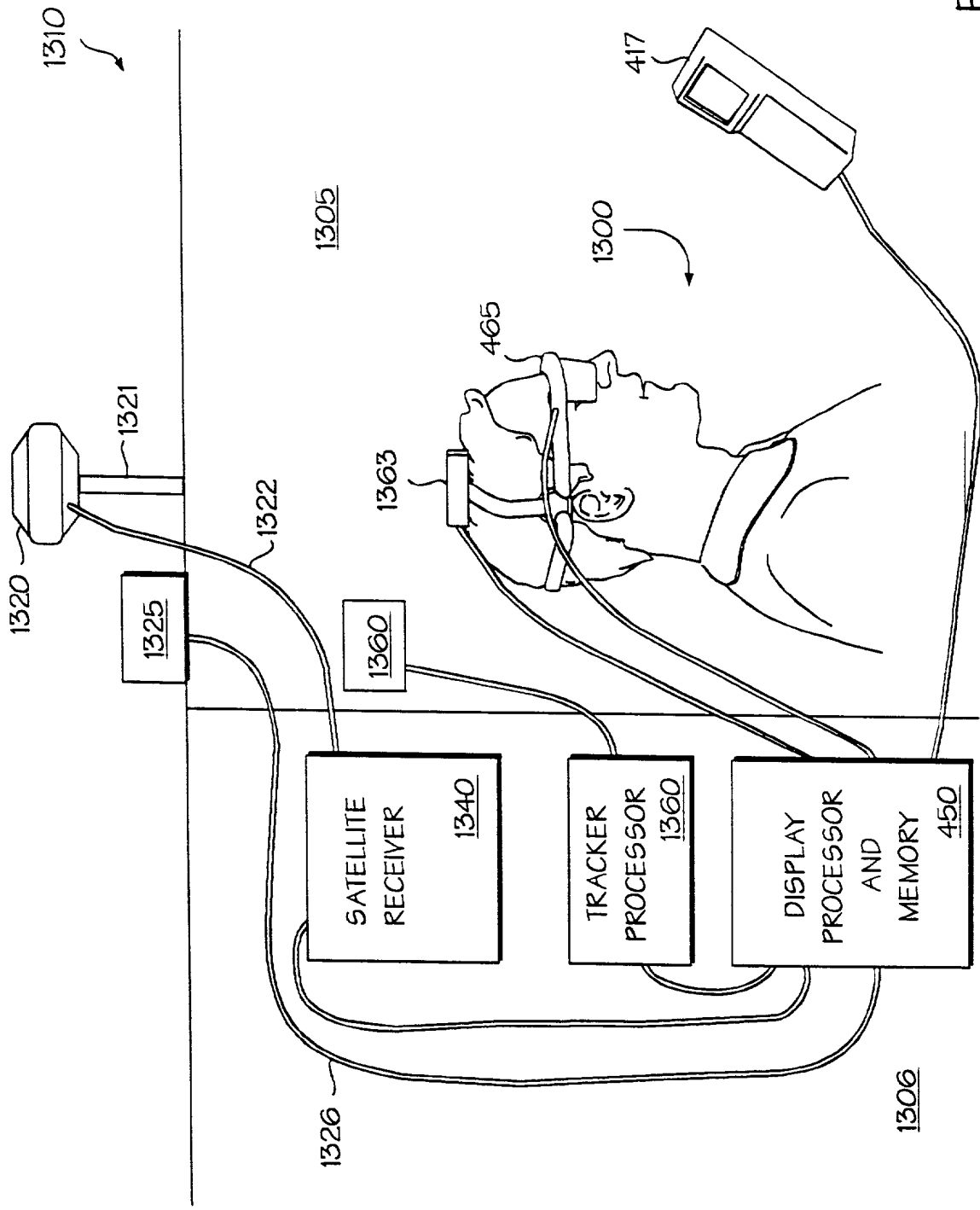


Fig. 27

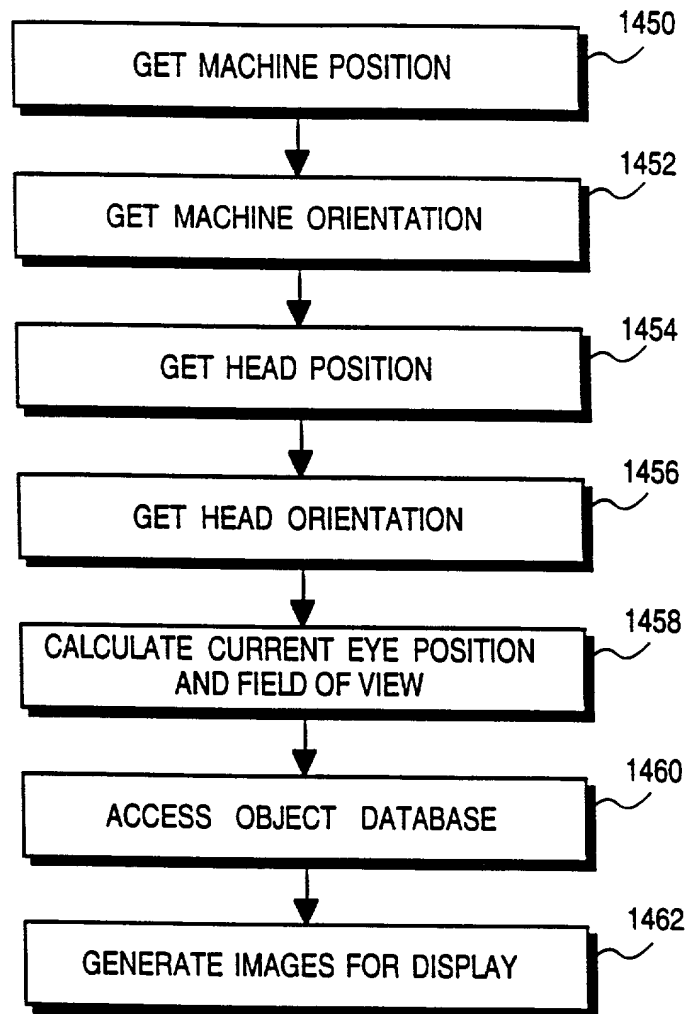


FIG. 30

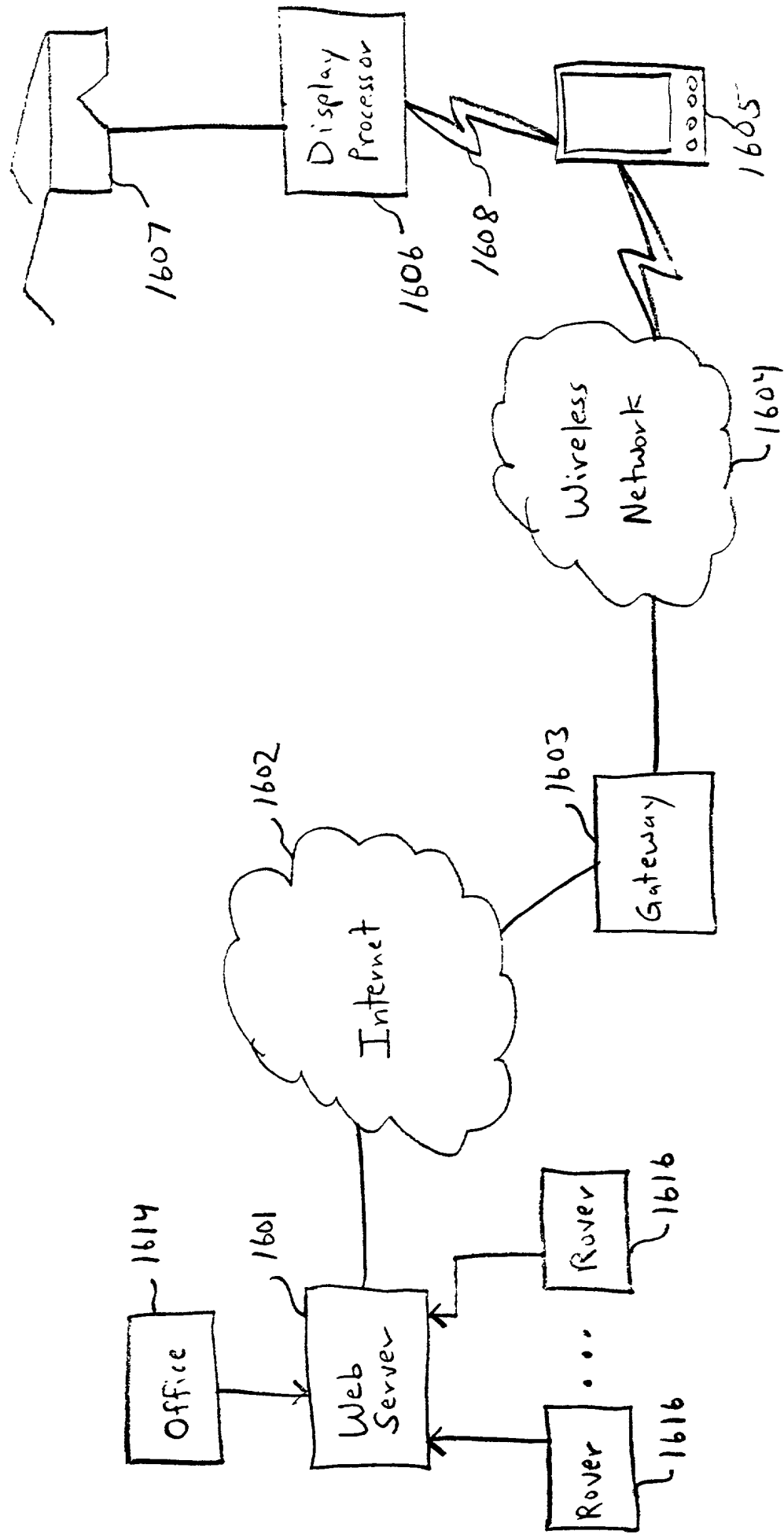


FIG. 31

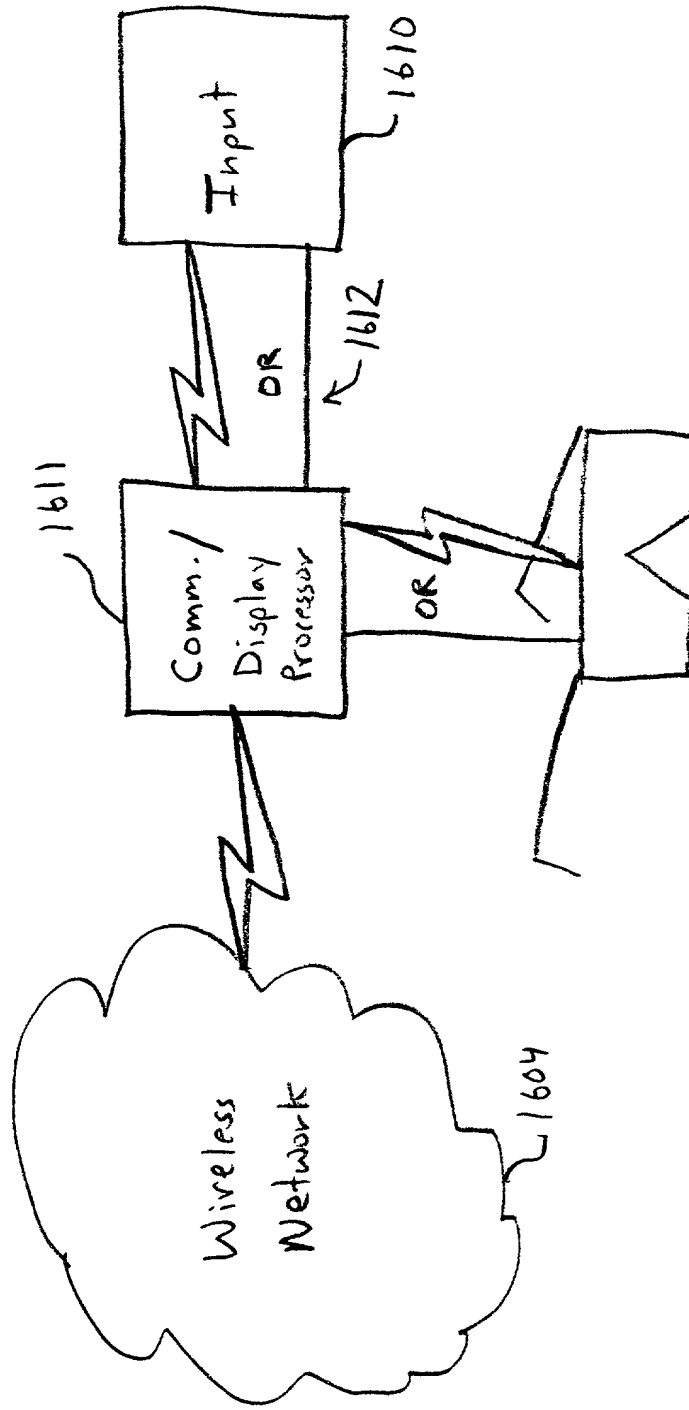


FIG. 32